

February 2, 2026

Ms. Emily Wentworth  
Community Planning Director  
Town of Hingham  
210 Central Street  
Hingham, MA 02043

SLR Project No.: 141.051021.00001

Client Reference No.: 21840.00008

**RE: Traffic Engineering Peer Review Responses  
Proposed Hingham Center for Active Living - Bare Cove Park Drive  
Hingham, Massachusetts**

---

Dear Ms. Wentworth:

SLR International Corporation (SLR) is in receipt of correspondence dated January 2, 2026, with comments from Jeffrey S. Dirk, PE, POTE, FITE of Vanasse & Associates, Inc. (VAI) of the review of the above-referenced project. We offer the following responses to the comments contained herein.

**December 2025 TIS**

CT1. The December 2025 TIS should be stamped and signed by the Professional Engineer (P.E.) in responsible charge for preparing the document or an affidavit should be provided by the P.E. attesting to their oversight in preparing the document and providing their Massachusetts P.E. Registration Number.

**RT1. The Revised TIS is stamped and signed by Nick Havan, PE, PTOE, ENV SP, Principal Transportation Engineer - SLR.**

CT2. Related to the new HCAL facility, it should be acknowledged that the MBTA provides The Ride paratransit services to eligible persons in accordance with the Americans with Disabilities Act (ADA). In addition, the HCAL provides transportation for Hingham residents to local medical appointments for well visits, essential shopping, local errands and to senior center programs. These accommodations will serve the new location of the HCAL and have been considered in the design of the facility.

**RT2. The Public Transportation section of the Revised TIS includes this information.**

CT3. Traffic volumes associated with the identified specific development projects by others (Beal Street senior housing and Bare Cove Pickleball Courts) should be generated separate from the general background traffic growth (1.0%) and added to the future condition traffic volumes. In particular, the pickleball courts will add turning volumes at the Fort Hill Street/Bare Cove Park Drive intersection and along Bare Cove Park Drive that may not be represented by the background traffic growth rate alone.

**RT3. For the traffic analysis of the Revised HCAL TIS, traffic volumes associated with the background developments have been generated separately.**

CT4. Traffic volumes associated with the South Shore Country Club pool complex should be included in the future year traffic volume projections.

**RT4. Traffic volumes associated with the South Shore Country Club pool complex have been included in the analysis.**

CT5. The Town of Hingham DPW should be consulted to determine if there are any planned roadway improvement projects in the area that may result in changes to traffic volumes, traffic patterns or operating conditions within the study area.

**RT5. Per the Town of Hingham DPW, there are no planned roadway improvement projects in Hingham that will impact roadway volumes.**

CT6. The Build condition traffic volumes should be revised to reflect the comments pertaining to the No-Build condition traffic volumes.

**RT6. The Build Condition traffic volumes have been revised accordingly.**

CT7. The traffic operations analysis should be revised to reflect the changes to the No-Build and Build condition traffic volumes and the response should clarify that the Synchro 11<sup>®</sup> capacity analysis software was used.

**RT7. The analyses have been revised accordingly.**

CT8. The impact of vehicle queuing on the Fort Hill Street approach to the Fort Hill Street/ West Street/South Street intersection on traffic operations at the Fort Hill Street/Bare Cove Park Drive intersection should be discussed as the predicted vehicle queue from the traffic signal was identified to extend beyond Bare Cove Park Drive.

**RT8. The impact is discussed in the revised TIS.**

CT9. In the absence of a speed measurement, it is customary to assess sight lines based on an approach speed of 5 to 10 miles per hour (mph) above the posted or statutory speed limit (25 to 30 mph in the case of Bare Cove Park Drive). In addition, the stopping sight distance along Bare Cove Park Drive should also be evaluated at both driveways in accordance with American Association of State Highway and Transportation Officials (AASHTO)<sup>1</sup> requirements. A review of the site plan and available imagery for Bare Cove Park Drive indicates that the stopping sight distance is provided and that the intersection sight distance can be provided with the trimming/removal of the trees and vegetation within the sight triangle areas. Given that the Town owns and controls the areas within which the trimming/removal will occur, the required minimum sight lines for an exiting motorist can be provided. Our comments pertaining to the sight line requirements follow.

---

<sup>1</sup> *A Policy on Geometric Design of Highway and Streets*, 7th Edition; American Association of State Highway and Transportation Officials (AASHTO); Washington D.C.; 2018.



**RT9. The sight line assessment was revised to use the statutory speed limit of Bare Cove Park Road, 30 mph.**

CT10. We are in agreement with the TDM measures that have been identified for the Project and the elements that will be included as a part of the Construction Management Plan (CMP), both of which should be included as a part of any conditions of approval that may be granted for the Project. In addition, the following recommendations should be considered that are directly associated with providing safe access to the Project site and facilitating improvements that are desirable independent of the Project:

- Fort Hill Street/Bare Cove Park Drive Intersection - As documented in the December 2025 TIS, all movements exiting Bare Cove Park Drive to Fort Hill Street are currently operating over capacity during one or more peak hours and motorist delays and vehicle queuing are expected to increase in the future independent of the Project. Improving this existing condition has been the subject of previous studies that have been undertaken by the Town and improvements have been implemented that include the widening of the Bare Cove Park Drive approach to provide two exiting travel lanes; providing left-turn lanes on the Fort Hill Street approaches; the addition of a sidewalk along the south side of Bare Cove Park Drive; and sign, pavement marking and sight line improvements. Independent of the Project, the Town has been advancing the planning for additional improvements along Fort Hill Street and at the Fort Hill Street/Bare Cove Park Drive intersection that include both traffic control measures to reduce motorist delays and vehicle queuing and mobility improvements to enhance pedestrian and bicycle access and safety. These improvements are expected to be a part of a Community One Stop for Growth grant application or other appropriate state grant application(s) that will be filed with the state.

In the interim and prior to the construction of the traffic control improvements at the intersection, the scheduling of events at the HCAL should be coordinated so as to avoid conflicts with the commuter peak periods at West Higham Station and when tournaments or other large events are scheduled within Bare Cove Park. When scheduling conflicts cannot be avoided, a police detail officer should be assigned to facilitate the movement of vehicles, pedestrians and bicyclists at the Fort Hill Street/Bare Cove Park Drive intersection as deemed necessary by the Police Chief, who should be consulted prior to the scheduling of overlapping events.

- Fort Hill Street/West Street/South Street Intersection - As documented in the December 2025 TIS, vehicle queuing on the Fort Hill Street approach to the Fort Hill Street/West Street/ South Street intersection currently extends beyond Bare Cove Park Drive at times during the peak hours. In order to address this existing condition, an optimal traffic signal timing and phasing plan should be developed for the intersection with consideration of the at-grade rail crossing of South Street.
- Safety - Existing trees and vegetation located along Bare Cove Park Drive should be reviewed and maintained. The AASHTO Guidelines for Geometric Design of Low-Volume Roads<sup>2</sup> recommends establishment of a minimum clear zone or recovery

---

<sup>2</sup> *Guidelines for the Geometric Design of Low-Volume Roads*, Second Edition; American Association of State Highway and Transportation Officials (AASHTO); Washington D.C.; 2019.



- area of 6-feet where such an accommodation can be provided “*at low cost and with minimum social or environmental impacts. .*”.
- Project Site Access - (see Site Plan comments)
  - Mobility - Consideration should be given to establishing the drop-off zone and the entirety of the drive opposite the drop-off zone as a raised table or using textured and colorized pavement to differentiate this area as a pedestrian zone. These features are shown on the rendering that was included as a part of the Site Plan but are not detailed on the plans.

**RT10. Agreed. Comment noted.**

### **Site Plan**

- CS1. A vehicle turning analysis (swept path) should be provided for the following design vehicles: trash/recycling vehicle (SU-40) and the largest anticipated delivery truck (WB-62 minimum for an articulated truck). The turning analysis should depict all maneuvers required to enter and exit the Project site from both directions and to access the required areas within the Project site.
- RS1. Additional vehicle turning movements have been added for the trash/recycling vehicle (SU-40). It is not anticipated that any delivery trucks will be larger than a box truck. However, if a WB-62 were to make a delivery to the site, the truck can turn at the end of Bare Cove Park Drive and back down the western entrance of the site to the delivery area.**
- CS2. The vehicle turning analysis for the Hingham Fire Department design vehicle indicates that the aerial portion of the design vehicle will swing beyond the curbline of the center island when entering the east Project site driveway. No objects should be placed in the island within the swing area that would exceed 7-feet in height or that would otherwise impede access by the fire truck.
- RS2. Comment noted. No objects are proposed in the swing area of the island. The fire truck will be able to make the turning movement as shown.**
- CS3. The Fire Department should confirm if an ambulance will be responding curbside to the entrance of the building. If so, the underside of the roof of the overhang should be at least 14.5-feet above the surface elevation of the pavement surface.
- RS3. The roof overhang provides enough height for an ambulance to drive under it. The current proposed height of the overhang is 14.5 feet minimum.**
- CS4. Consideration should be given to provide double-yellow centerline pavement markings on the approaches to the west driveway within the Project site for a minimum distance of 100 linear feet.



**RS4. A double-yellow centerline has been added for a distance of 75 feet. The distance of 100 feet would conflict with intersection of one of the parking aisles and may cause confusion.**

CS5. The sight triangles for both driveways should be added to the Site Plan. The sight triangles should be established with the driver exiting the Project site at a point 14.5-feet from the edge of the traveled-way on Bare Cove Park Drive and a minimum sight distance of 200 feet. In addition, the following note should be included on the plan sheet that depicts the sight triangles: "Signs, landscaping and other features located within sight triangle areas shall be designed, installed, and maintained so as not to exceed 2.0-feet in height. Snow accumulation (windrows) located within sight triangle areas that exceed 3.5-feet in height or that would otherwise inhibit sight lines shall be promptly removed."

**RS5. Sight triangles and notes have been added to Sheet LA.**

CS6. A note should be added to the Site Plan stating: "All Signs and pavement markings to be installed within the Project site shall conform to the applicable specifications of the latest edition of the Manual on Uniform Traffic Control Devices (MUTCD).<sup>3</sup>"

**RS6. A note has been added to Sheet SPM.**

CS7. "No Left Turn" and "One-Way" signs should be installed along Bare Cove Park Drive opposite the exit from the east Project site driveway.

**RS7. Additional signage has been added to Sheet SPM.**

CS8. Consideration should be given to establishing raised islands with landscaping to define the edge of the traveled-way and parking areas rather than the striping that is proposed at the west end of Bare Cove Park Drive. This is particularly important where directional and regulatory signs are to be installed.

**RS8. Based on consultation with the Town Engineer and DPW, raised islands are not desirable along or within Bare Cove Park Drive, as they would create issues with snow plowing, and can impede upon the flow of stormwater.**

---

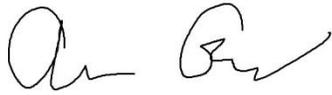
<sup>3</sup> Federal Highway Administration, op. cit. 3.



Please feel free to contact us with any additional questions.

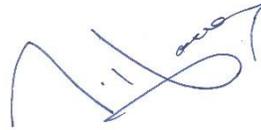
Regards,

**SLR International Corporation**



**Andrew Goralski, PE**  
Associate Civil Engineer  
agoralski@slrconsulting.com

141.21840.00008.f226.ltr.2



**Nick Havan, PE, PTOE, ENV SP**  
Principal Transportation Engineer  
nhavan@slrconsulting.com

